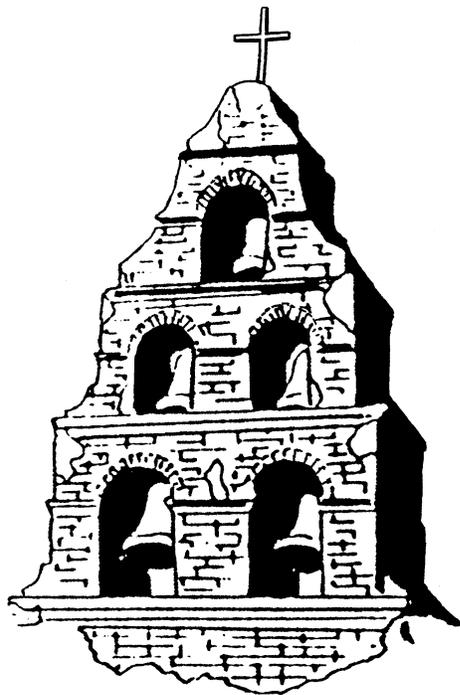


San Miguel

Community Design Plan



DEPARTMENT OF PLANNING AND BUILDING
COUNTY OF SAN LUIS OBISPO
SAN LUIS OBISPO, CA 93408

San Miguel

Community Design Plan

ADOPTED BY
THE SAN LUIS OBISPO COUNTY
BOARD OF SUPERVISORS
APRIL 8, 2003

DEPARTMENT OF PLANNING AND BUILDING
COUNTY OF SAN LUIS OBISPO
SAN LUIS OBISPO, CA 93408

SAN MIGUEL COMMUNITY DESIGN PLAN

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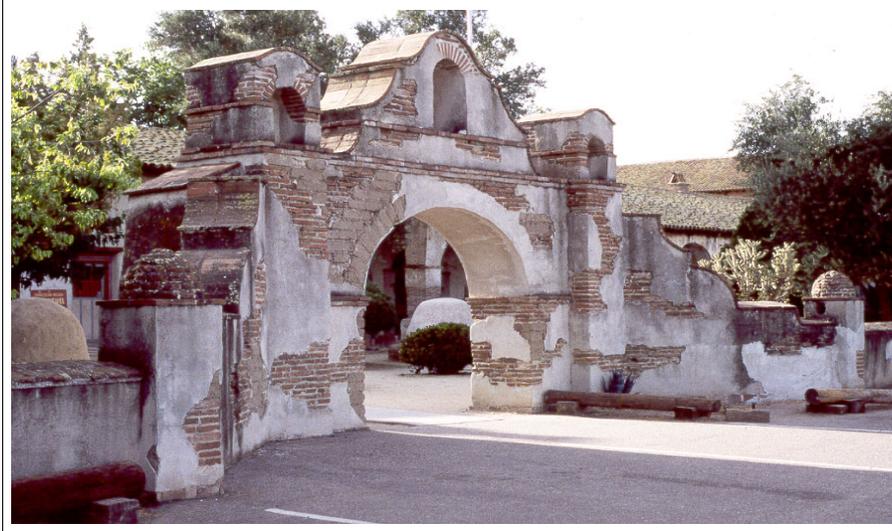
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San Miguel Community Design Plan



A. Introduction

Purpose of the plan. In opinion surveys and workshops, residents of San Miguel have expressed a desire for specific standards, guidelines and programs to address certain characteristics of the community that have frustrated the development of a wider range of housing opportunities and a vigorous local economy. The Community Design Plan is intended to provide a framework that will ensure that new development adds value to the community while preserving the positive features of San Miguel's character.

How the plan will be used. This plan is intended to influence development in San Miguel so that the form, character and historical resources of the community are enhanced. The plan is available to prospective developers, who will be able to create development plans that are consistent with the community's intent. The plan provides the community with a way to measure the value of individual proposals as they are presented for local review.

The plan contains both "standards" and "guidelines". Standards are mandatory elements that must be included in any development. Guidelines are advisory and discretionary, indicating desirable qualities that a development proposal should include unless there is a compelling reason not to do so - for example, when a proposal includes features that would result in a superior project, even though it does not meet all the applicable guidelines. The guidelines are also used by County staff, the Planning Commission and

the Board of Supervisors in the review development projects. The development review process makes a careful examination of a project's quality of site planning and architecture, as well as such details as signage, lighting and drainage. The purpose is to ensure that every new development will carefully consider the community context in which it will exist and make a conscientious effort to achieve a compatible relationship to the natural setting, neighboring properties and community design objectives.

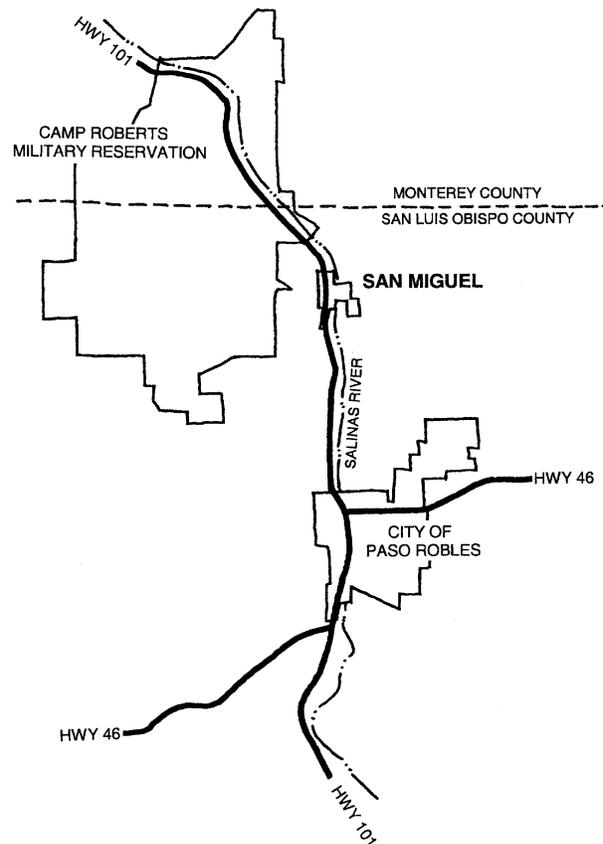
How the plan relates to the County General Plan. The *County General Plan* provides a comprehensive framework of policies and regulations to protect and manage local resources and community development. The *Land Use Element* is one of several documents that comprise the General Plan. It defines land use categories and specifies the allowable uses within each category. The *Land Use Element* is made up of 15 "Area Plans" that, together, cover the entire geographic area of San Luis Obispo County. The area plans contain land use and circulation policies and programs that address issues unique to a particular planning area. San Miguel is located in the region covered by the *Salinas River Area Plan*. The *Land Use Ordinance* contains development regulations that are applicable county-wide, as well as standards and guidelines for local communities that may be different than the county-wide regulations.

The San Miguel Design Plan is adopted as part of the *Land Use Ordinance*, to provide design-related policies, programs and standards that supplement those in the area plan. The standards and guidelines in the San Miguel Design Plan have the effect of law and they take precedence over any conflicting *Land Use Ordinance* standards.



B. San Miguel Community Assessment

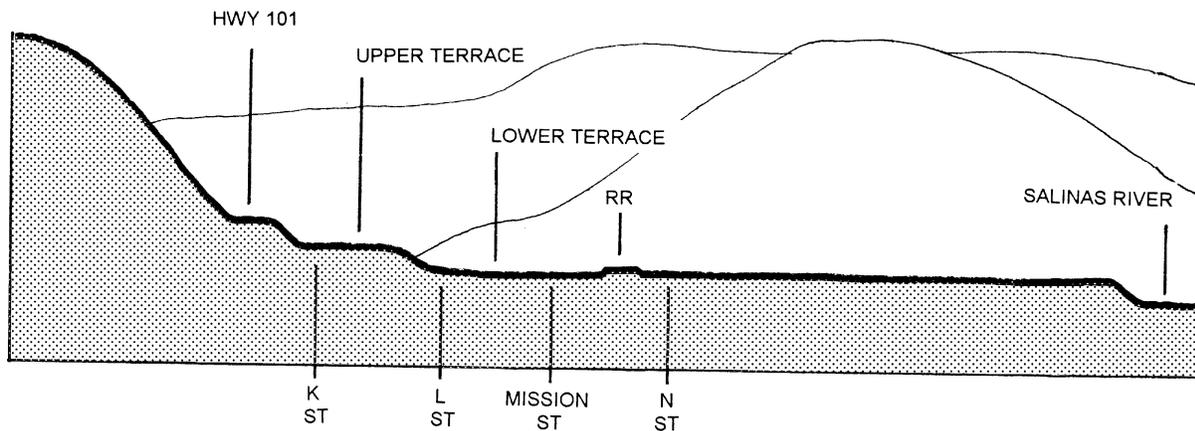
A brief history of San Miguel. San Miguel's modern history begins with the founding of Mission San Miguel Arcangel in 1797 in territory inhabited by the Salinan people. The town grew up south of the mission on the west bank of the Salinas River. The railroad arrived from the north in 1886, providing convenient access to the San Francisco market for San Miguel's production of cattle and grain. When the original town was destroyed by fire in 1887, the new San Miguel was built near the terminus of the railroad line, north of the mission. During World War II, San Miguel became the off-duty retreat for 45,000 troops stationed at Camp Roberts, and the town's population grew to 4,000. Camp Roberts was de-activated in the late 1950's, and the local economy suffered. Many commercial and residential buildings began a cycle of vacancy, abandonment, deterioration and demolition, leaving vacant lots where once there had been homes and thriving businesses. Also in the 50's, the new freeway was constructed along the town's western edge, bypassing San Miguel's commercial district and adding insult to injury. For forty years, the community has hoped for a return of economic vitality, but its former prosperity has remained a memory. Some new homes have been built, but not enough to create a market for new local business.



Physical setting. San Miguel is transected by several parallel features that define its boundaries and provide organization to the area within. On the west, the community is defined by Highway 101 and the steep hillside along the highway's western edge. The Salinas River defines San Miguel's symbolic eastern boundary, although the community's Urban Reserve Line extends east of the river to encompass the San Lawrence Terrace development. The Union Pacific railroad tracks run through the middle of town, almost equidistant from the highway and the river.

Topographically, San Miguel consists of two terraces connected by a steep slope. The upper terrace extends from Highway 101 to a point east of the alley between K and L Streets. The lower terrace extends from L street to the Salinas River. The upper terrace has views of the hills east of the river and fewer surface drainage problems than other parts of town, but has greater exposure to highway noise.

Several locations within the lower terrace experience seasonal ponding of stormwater.



However, homes in this area are somewhat sheltered from highway and railroad noise. The older, more fully developed part of town lies between the highway and the railroad property. This part of San Miguel is laid out as a grid of blocks measuring 400 feet in the north/south direction and 320 feet in the east/west direction. A north/south alley divides each block into 150 foot deep parcels. East of the railroad, only the land fronting on N Street from 11th Street to 15th Street was originally subdivided in this manner. The remainder of the land has remained without a formal network of streets and lots. New subdivisions have provided lot patterns and street segments on a piecemeal basis. These layouts are determined primarily by the configuration of the pre-subdivided parcels, rather than by recognition of community design and circulation issues.

Gateways. The most commonly used approach to San Miguel is from Highway 101. For people arriving from the south, entry points at South Mission Street and Tenth Street are well-defined by views of the mission buildings and the distinctive bell tower at the southern end of the property. For motorists arriving from the north, San Miguel is almost invisible. The North Mission Street connection to Highway 101 appears well before any visual evidence that the community exists. Farther south, topography and mature trees along the east edge of the highway make it difficult to see much of San Miguel from the southbound lanes. Where motorists exit Highway 101 at Tenth and K Streets, there are no visual links to the downtown area or to the mission, except as provided by directional signs. For those

approaching from the north or south along Mission Street, there is no sense of arrival at the downtown business district. The mission, itself, is a landmark for motorists arriving on Mission Street from both directions.

Circulation. San Miguel is a “walkable” community. Except for the San Lawrence Terrace area across the river, approximately 60 percent of the population lives within a quarter mile of the post office on Mission Street; 100 percent within a half mile. Many children walk or ride their bikes to school. The street grid west of Mission Street provides motorists with a variety of routes and avoids concentration of local traffic on one street as opposed to others. Through traffic uses Mission Street. 11th Street and 14th Street (River Road) are the only two streets that cross the railroad tracks. These signalized crossings are used by both pedestrians and vehicles. River Road leads to the bridge across the Salinas River and the agricultural area to the east. Speeding is common on Mission Street. The wide right-of-way, the absence of buildings on the east side of the street and the absence of a sense of entry to the downtown business district give through traffic little reason to slow down.

Residential development. Many of San Miguel’s residential areas have an historic character that is only possessed by communities that were developed before the advent of mass-produced housing. This is especially true of the older neighborhood between the highway and Mission Street. Each house has its own “look”. Setbacks from the street vary from lot to lot. Architectural detailing, though modest, is distinctive when compared to many contemporary examples. Some buildings are of genuine historical interest. However, time has taken a



toll. Maintenance deferred over a span of many years has contributed to an impression of shabbiness in some parts of town. Many homes are small, and indoor storage space is often inadequate to accommodate all the possessions that a typical household accumulates. When these items overflow into the yard, they also contribute to the community’s untidy appearance.

The older neighborhoods are distinguished by the presence of many mature trees that provide shade and a sense of visual separation from the highway.

Commercial development. The remnants of San Miguel's business district are concentrated in the three blocks from 11th Street to 14th Street, across Mission Street from the site of the old railroad depot. Most of the buildings in and around the business district have existed there since the 1940's, with some dating to the early 1900's.

In contrast to San Miguel's residential areas, the commercial district has few shade trees. A row of huge eucalyptus trees defines the eastern edge of Mission Street from 11th Street to 12th Street. Street trees have been planted from 12th to 14th Streets, also on the east side of Mission Street, but their growth has been slower than anticipated. Preserving and rehabilitating the historic buildings and improving the pedestrian environment with landscaping and other features will be important factors in the resurrection of a vigorous local economy.



San Miguel's population growth has been relatively modest over the last several decades, when compared to the rapid growth of Paso Robles, a ten minute drive away. Understandably, entrepreneurs looking for market support for new businesses have focused on Paso Robles. San Miguel needs the spark of something "new" to kindle the interest of businesses in locating in the community. The community design plan can provide the spark, by awakening people to San Miguel's potential to develop into an attractive and liveable community.

Vacant or under-utilized land. One of the most significant features of San Miguel is that its main street - Mission Street - has buildings on only the west side of the street. With a few exceptions, the east frontage of Mission Street is vacant. Much of this vacant land is owned by the railroad, for which land development has not been a high priority. The railroad has sold several shallow parcels fronting on Mission Street to other private owners, but these have remained undeveloped.

Development on the west side of Mission Street has dwindled during the past thirty or forty years, as some buildings in poor condition have been removed and not replaced.

East of the railroad tracks, the west side of N street remains undeveloped, in part because the right-of-way needed by the railroad leaves an even more shallow developable strip than on the west side of the tracks. East of N Street and south of River Road, the absence of a coherent street system has acted as a deterrent to development.

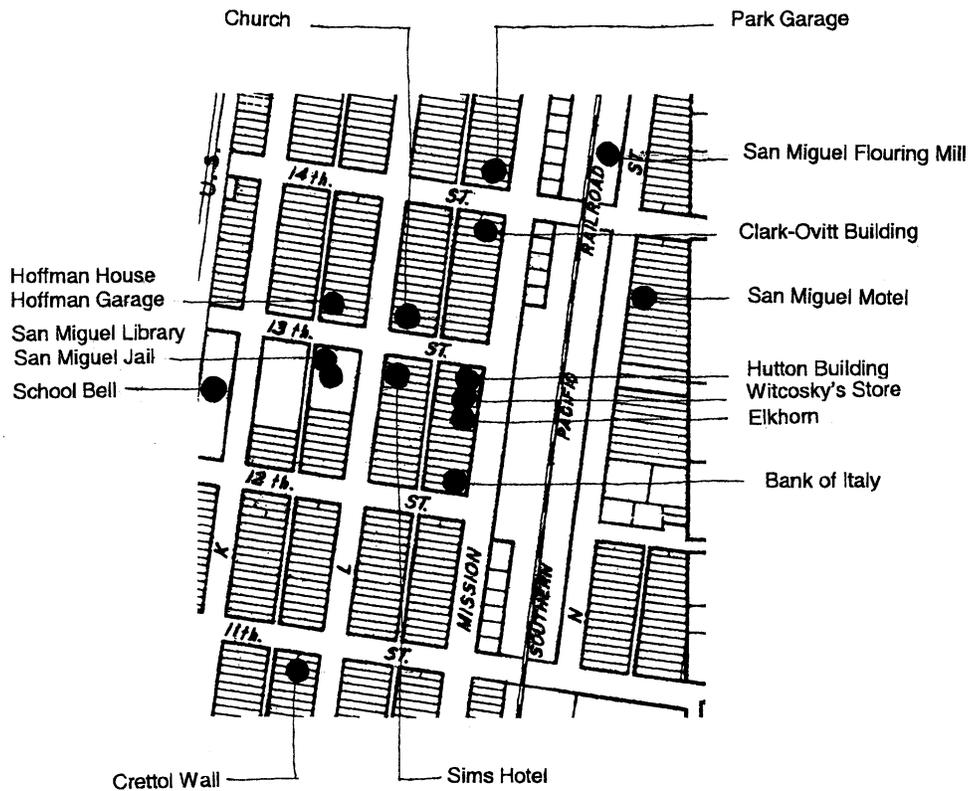
Historic Resources. In addition to the mission and the Caledonia adobe, San Miguel has many commercial and residential structures built during the period from the late 1800s through the 1950s that contribute to the community’s unique historic character. It is important that these historic buildings be maintained in good condition. Otherwise, they may deteriorate to the point at which demolition becomes a realistic alternative. Many buildings with historic value have already disappeared. A survey should be conducted to document San Miguel’s historic resources. This information can be used to establish eligibility for grants and loans to help pay for rehabilitation efforts, and to qualify for building permit standards specifically designed to encourage the preservation of a building’s historic character. The following list and map include historic buildings identified in a self-guided tour pamphlet published by the San Miguel Business Association:

San Miguel Historic Resources, a partial list

Property Name	Location	Year Built	Comment
San Miguel Motel	13 th and N	Late 1800s	Originally, “La Favorite” Hotel
San Miguel Flouring Mill	14 th and N	Late 1800s	Oldest business in continuous operation
Park Garage	14 th and Mission	1922	Once a Hupmobile dealership
Clark-Ovitt Building	14 th and Mission	1927	Formerly, telephone office, bus depot
Church	13 th and L	1887	
Sims Hotel	13 th and L	1893	Later, the Makin Rooming House
Hoffman House	13 th , K / L	Early 1800s	Later, the Maxwell Rooming House
Hoffman Garage	13 th , K / L	Early 1800s	C.E.Hoffman designed and built town’s first electric light company
San Miguel Library	13 th , K / L	Early 1940s	Also used as a courthouse
San Miguel Jail	Behind library	WWII era	
School Bell	K, 12 th / 13th	1888	Site of first schoolhouse, removed to construct Highway 101

San Miguel Historic Resources, continued

Property Name	Location	Year Built	Comment
Crettol Wall	11 th and L	1958	Built by Jesse Crettol, Swiss stonemason who helped restore the Mission and built the stone bell tower in the Mission Cemetery
Bank of Italy	12 th and Mission	1917	Later, Bank of America
Elkhorn	Mission 12 th /13 th	?	
Witcosky's Store	Mission 12 th /13 th	Early 1900s	Original building
Hutton Building	Mission and 13 th	1940s	Has housed various businesses



San Miguel Historic Resources

C. Community Goals

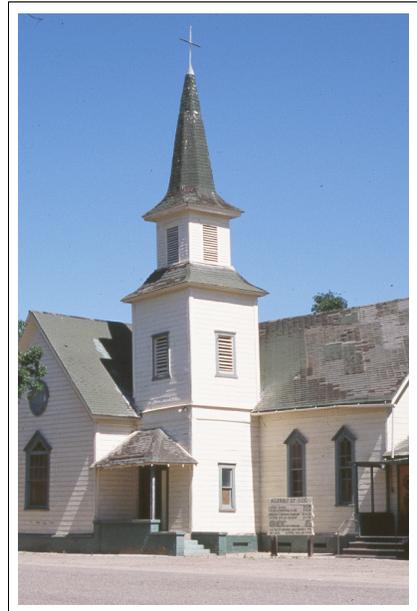
Beginning with a community survey conducted in 1998 and in subsequent public meetings sponsored by the San Miguel Advisory Committee, residents of San Miguel have identified two primary goals that provide the foundation for the community design plan. Achievement of these goals will have a beneficial effect on many of the issues about which residents have expressed their concerns.

GOAL 1: Improve the community's ability to attract commercial development and housing for people of all income levels.

Residents of San Miguel want to encourage the development of a wider range of housing opportunities for all income levels. Attracting more middle- and upper-income housing will require a coordinated effort to "polish" the image of the community in order to provide an appropriate context for new development. The visual impression created by the condition of private property and by the public streetscape are equally important components of San Miguel's image.

GOAL 2: Promote tourism to support local business.

Mission San Miguel is the community's most significant asset, attracting thousands of visitors annually. Many visitors see only the mission, itself, and resume their travels, unaware that a visit to downtown San Miguel could enrich their historical experience. Promotion of the community's other historical resources can provide an opportunity for local business owners to capture a larger share of county tourism revenue. Antique shops, cafes and restaurants appeal to the visitor market and add a measure of economic vitality that can encourage the establishment of new businesses to serve local residents as well as visitors.



D. Objectives and Strategies Designed to Achieve Community Goals

1. Create a more desirable context for new residential development

- a. **Vigorous enforcement of land use and environmental health regulations.** The county should maintain a program of pro-active enforcement of land use and health regulations.
- b. **Education program to emphasize the importance of maintaining community appearance on a year-round basis.** A leaflet should be created, based on the City of San Luis Obispo's "Checklist for Good Neighbors", for distribution to all households. The leaflet should be given to all new residents. The checklist, in both Spanish and English, should contain a summary of county, CSD and other local regulations regarding weed and trash in yards, animals, storage of vehicles, burning of trash, etc. It should also contain a section devoted to maintenance of alleys and regulations for parking in alleys, and should include a list of phone numbers to call for various public agencies.

If you are concerned about how your neighbor treats his property....

First
Try having a heart-to-heart. Politely let your neighbor know what is causing you trouble.



If for some reason you can't talk to your neighbor, try leaving a copy of this pamphlet in a safe place (tucked under the door, rubber-banded to the doorknob - not in the mailbox - that's illegal).



If that doesn't work,
Work through your neighborhood association's, Neighborhood Action Team, if you have one. (If you don't, you might consider starting one.)



And if you still don't get results,
Call the numbers listed on the back.



Problem	Call	Number
Parks: restroom problems broken play equipment		
Disabled cars, parking in yard		
Illegal burning, overgrown weeds		
Sewer back-up or excessive odor		
Converted garage, illegal occupancy		
Loose or noisy animals		
Noise: parties or other disturbances		
Recycling, water conservation		
RVs used as residence		
Paving of front yard, home occupations		
Debris or storage in yard		
Water meter or main leaks		
Sidewalk conditions, missing or damaged street signs		
Bus service: within SLO out of city		

Checklist for Good Neighbors



- c. **Clean-up programs.** The community should sponsor periodic clean-up programs similar to the successful effort in 1998. Clean-up campaigns have the obvious benefit of ridding the community of visible refuse; in addition, they have an educational benefit because they reinforce the concept of a community standard. They are envisioned as a largely volunteer effort, with costs offset by local fundraising and with assistance from the county as staffing and other resources allow. Another program would target specific neighborhoods. Volunteers would provide cleanup assistance to each neighborhood in turn. This type of program is visible to the public throughout the year, and the educational effect is continuous.
- d. **Alleys.** The original townsite was laid out with 20 foot wide alleys. The alleys serve as utility corridors, routes for trash collection and access for emergency and private vehicles to the rear of parcels with frontage on K, L and Mission Streets. Because of the steep slope along the west side of L Street, from 10th to 14th Street, many homes were built on the flatter, higher western end of these L Street lots, with their primary access from the alley rather than the street. All the vehicular activity associated with typical residential units is concentrated in the alley, which is too narrow to accommodate it. Future residential development with an alley as its primary access should be required to have two on-site parking spaces in addition to those normally required, in order to compensate for the absence of street parking. For all development, parking in alleys should be prohibited and strictly enforced.

2. Concept plans, standards and guidelines for new residential development

- a. **Circulation concept for east side.** Streets are designed for pedestrian circulation as well as for vehicles. The continuity of the street system provides the physical connections and opportunity for personal interaction that are essential to a sense of community. The portion of the community between the tracks and the river lacks such a network of streets. The street system concept shown on the following page can provide the necessary continuity. Adoption of this street system as a component of the community design plan helps potential developers envision how their particular parcel or subdivision relates to the whole community. The emphasis is on using the street system to create a “community” rather than a collection of separate residential enclaves.



Street system provides more than one route to travel from one point to another.

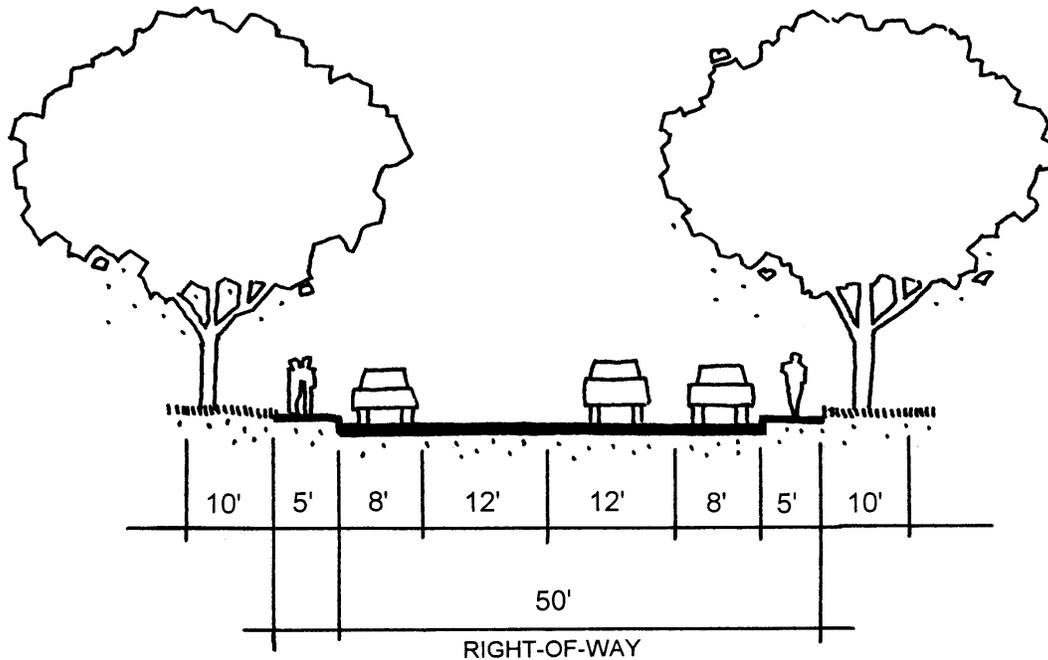
A second north-south route is provided between N Street and the river.

East Side Circulation Concept

Two north-south routes connect the southern area to 11th Street and River Road to the north.

Note: Roadway alignments may be modified to address such issues as property boundaries and topography

- b. Street cross-sections.** These diagrams give the dimensions of new residential streets, indicating how the public right-of-way will be used - how much width for sidewalks, planting strips, traffic lanes, whether there will be parking allowed on one or both sides of the street, and the design of curbs and gutters. The preferred cross-section for new residential streets in San Miguel is shown in the following diagram .



RESIDENTIAL STREET

- c. Guidelines and standards for single-family development.** Single family residential infill development should be designed for compatibility with the existing pattern of development. New subdivisions should provide a setting that can comfortably accommodate a variety of housing sizes and designs for residents of all income levels. Guidelines and standards for single-family development are found in the Implementation section of this plan. All new subdivision development in the Residential Single Family land use category is required to provide curb, gutter and sidewalk in accordance with the street cross section.

- d. Guidelines and standards for multi-family development.** Within the San Miguel urban reserve line are approximately 37 acres of land designated for multi-family residential development (RMF). About one fourth of this amount is vacant; the balance is occupied by existing development. There are several locations where new multi-family development will face a single-family neighborhood across the street or where the two will share a common property line. It is important that new RMF development should be visually and functionally compatible with the dominant pattern of single-family development in San Miguel, reflecting the neighborhood character, streetscape and scale. The adoption of appropriate standards and guidelines will help to achieve this objective. Specific guidelines and standards for multi-family development may be found in the **Implementation** section of the San Miguel community design plan.
- e. Guidelines and standards for new development on the west side of N St, from 11th to 14th.** The land between N Street and the railroad tracks is designated for industrial use. Because of its highly visible location and because this industrial property is so closely integrated with residential and commercial service districts, its development will have a significant impact on those adjoining districts. Also, the design of new buildings in this area should consider the appearance of the facade that faces the railroad track. These facades will be visible from the Mission Street side of the track. They should include the same design elements as the buildings' street facades. Standards and guidelines similar to those applying to non-residential development elsewhere in the community should be followed to ensure a positive visual and functional relationship to other development in the area.

Because of the railroad's need to preserve right-of-way to service the tracks, development on the west side of N Street will be limited to about 50 feet in depth. This suggests that new development may prefer other locations with greater flexibility for site design. The N Street frontage may have potential for some non-industrial use, such as public recreation. For example, facilities such as tennis and basketball courts, or a skateboard park could be developed within the 50 foot strip.

3. Improve the appearance of the Mission Street corridor.

The image of San Miguel is based primarily on the image of Mission Street. It is essential that development of the streetscape of this corridor, including public development within the street right-of-way and private development along its edges, should contribute to a positive image for San Miguel. The image must reinforce the town's historic character. It must convey a sense of a community that welcomes visitors. And, it must possess a congenial pedestrian environment that encourages people to linger. For new private development, these objectives will be achieved through observance of the standards and guidelines found in the **Implementation** section. Enhancement of public areas should include benches, bicycle racks, trash receptacles and pedestrian-scale street lights, possibly of a design that evokes the historic mission bell signs that marked El Camino Real.



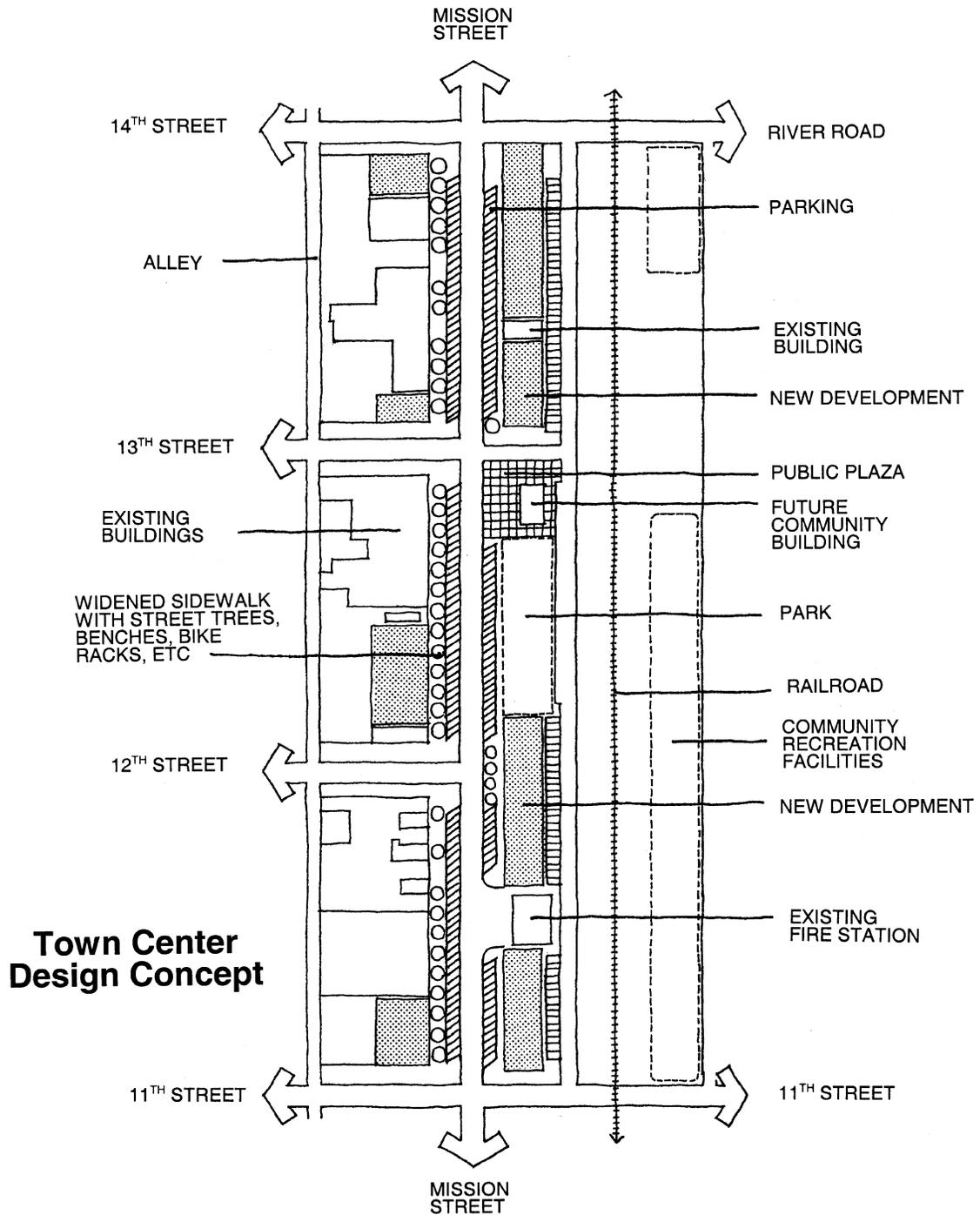
4. Improve the downtown area so it can become a secondary destination for visitors to the mission.

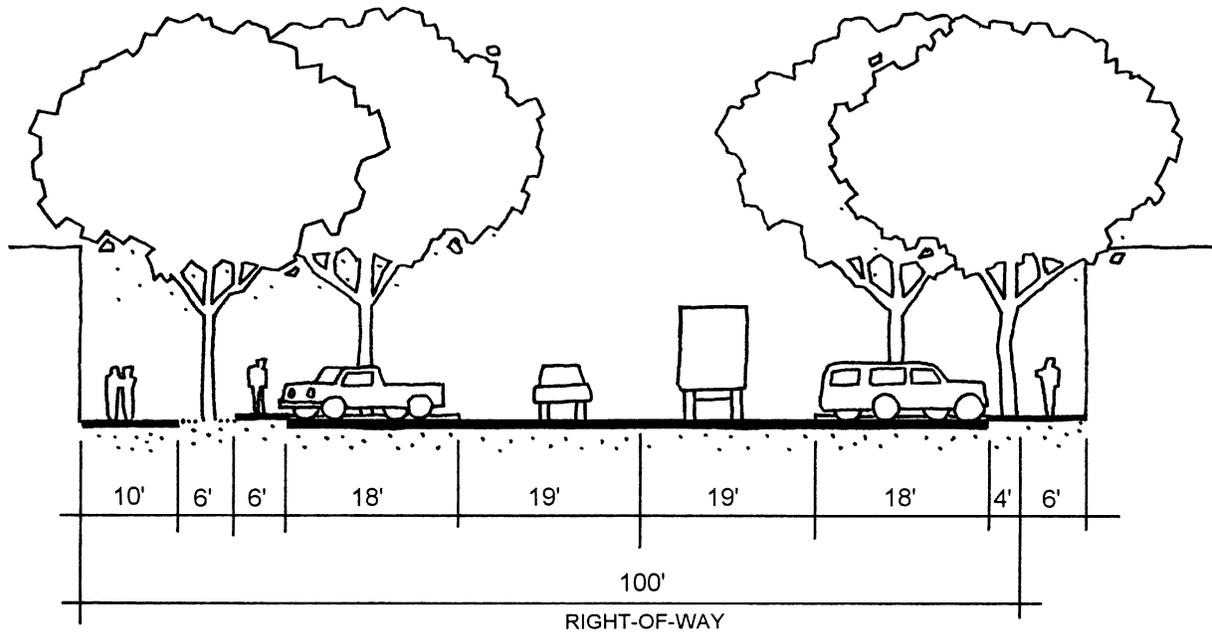
- a. **Implement a plan for a “town center”.** The Town Center plan should include a significant public open space area. This open area should be located between 12th and 13th Streets, on the east side of Mission Street, an area formerly occupied by the original railroad depot. Although the depot is gone, the design of this new public space can include some of the same elements as the original historic development - lawns, shade trees and a bandstand or gazebo. Picnic tables and a visitor center with historic exhibits and well-maintained public restrooms would help attract visitors from the mission to the downtown area. The county should seek funding to implement the town center concept and expand the potential for implementation of improvements throughout the downtown area. Funding will come primarily from grant programs, which typically require local matching funds of approximately 10 to

12 percent of the total grant amount. In addition, arrangements for maintaining the improvements once they are constructed must be determined. Landscape elements, public restrooms, trash receptacles and lighting all require some level of maintenance. The preferred approach would make the maintenance function the responsibility of a business improvement district, paid for by an annual assessment of district members. It may also be possible to create an arrangement whereby maintenance responsibilities are shared by some combination of volunteers, a local business association, community services district and the county. A concept plan for the San Miguel Town Center is shown on page 18.

- b. Reduce vehicle speed on Mission Street.** Studies of driver behavior indicate that drivers instinctively drive more slowly on streets they perceive as “narrow” than they do on streets they perceive as “wide”. By narrowing the apparent width of Mission Street in the downtown area, motorists will be encouraged to slow down and the street will become safer for pedestrians and bicyclists. The Town Center plan can accomplish this objective by the inclusion of the following design elements:

- ✓ Large canopy street trees, which also provide shade for pedestrians;
- ✓ Diagonal parking, which has the added benefit of increasing the supply of on-street parking spaces;
- ✓ Sidewalks at the end of each block extended to the edges of the vehicle travel lanes.
- ✓ Textured crosswalks that create an audible “rumble” effect to remind motorists that they need to slow down in areas of increased pedestrian activity.
- ✓ Class 3 bike lanes make the entire traffic lane accessible to bicyclists, enabling them to avoid cars backing out of parking spaces.





MISSION STREET
11TH STREET TO 14TH STREET, NORTHBOUND

- c. **Guidelines and standards for new downtown structures.** The core of San Miguel's downtown includes the block of historic buildings at the corner of 13th Street and Mission, as well as other older buildings along Mission between 11th and 14th Streets. Many of these buildings date from the early 1900s and contain architectural elements typical of that period of California's history. The community intends that new downtown buildings should be compatible in function and appearance with the existing buildings. Compatibility is to be achieved, not by requiring that new buildings adhere to a particular design theme or style, but by observance of guidelines and standards dealing with site design, building height and proportion, and pedestrian-scale architectural details. Examples of appropriate styles include most variations commonly seen from the late 1800s through the 1940s and styles that incorporate design elements from the mission.

The design of new buildings on the east side of Mission Street in the downtown area should consider the appearance of the facade that faces the railroad track. These facades will be visible from the N Street side of the track. They should include the same design elements as the buildings' street facades.



Standards and guidelines for downtown development may be found in the **Implementation** section of this community design plan.

- d. **Encourage “mixed use” development.** The county’s Land Use Ordinance allows residential uses to be located on the second floor above ground floor commercial uses. This kind of “mixed use” development is appropriate for downtown areas. The residential units generate pedestrian activity which can help to animate the streetscape. Also, residents can provide a “neighborhood watch” function during times when businesses are closed. San Miguel encourages this kind of mixed use development.

In order to encourage commercial development, the community wishes to expand the uses allowed in the Commercial Retail land use category in the downtown core area. New allowed uses could include laundromats and vehicle repair & service establishments, if their design complies with standards that would ensure their compatibility with the objective of maintaining a pedestrian-friendly environment, and with other standards and guidelines for downtown San Miguel. Implementation of this proposal will require a separate amendment to the *Framework for Planning* portion of the county general plan, in addition to the amendment to the Salinas River Area Plan incorporating the San Miguel Community Design Plan.

- e. **Locally appropriate parking standards.** Motor vehicles and the land they take up for parking and maneuvering are generally incompatible with pedestrian activities. San Miguel’s downtown core area must become a pedestrian-friendly area if it is to achieve it’s economic potential. Adequate parking must be provided, but it must not be allowed to compromise the pedestrian environment, visually or functionally.

Sidewalks on Mission Street should not be interrupted by driveways. Parking requirements should consider the fact that eighty percent of San Miguel's residents live within a quarter mile of the downtown core - convenient walking or biking distance. Tourists and other visitors will park somewhere in the core area and go from place to place on foot.

For these reasons, the design plan includes reduced parking requirements for new land uses in the downtown core. However, reduction of parking requirements in the downtown area must consider the need to provide adequate parking for existing uses such as the post office.



5. Capitalize on the presence of historic resources.

a. Identify historic resources. Initially, conduct a local inventory of historic resources. Investigate the desirability of seeking some level of official designation by the county or state. This can be accomplished by local volunteers with assistance from the county.

b. Design a walking tour of San Miguel's historic areas and promote it as a visitor activity. Prepare an attractive pamphlet for distribution to visitors to the mission.



- c. **Create a program for assisting owners of historic buildings.** In some cases, low-interest loans may be available to finance improvements to historic properties. The county can assist in identifying possible sources of funding.

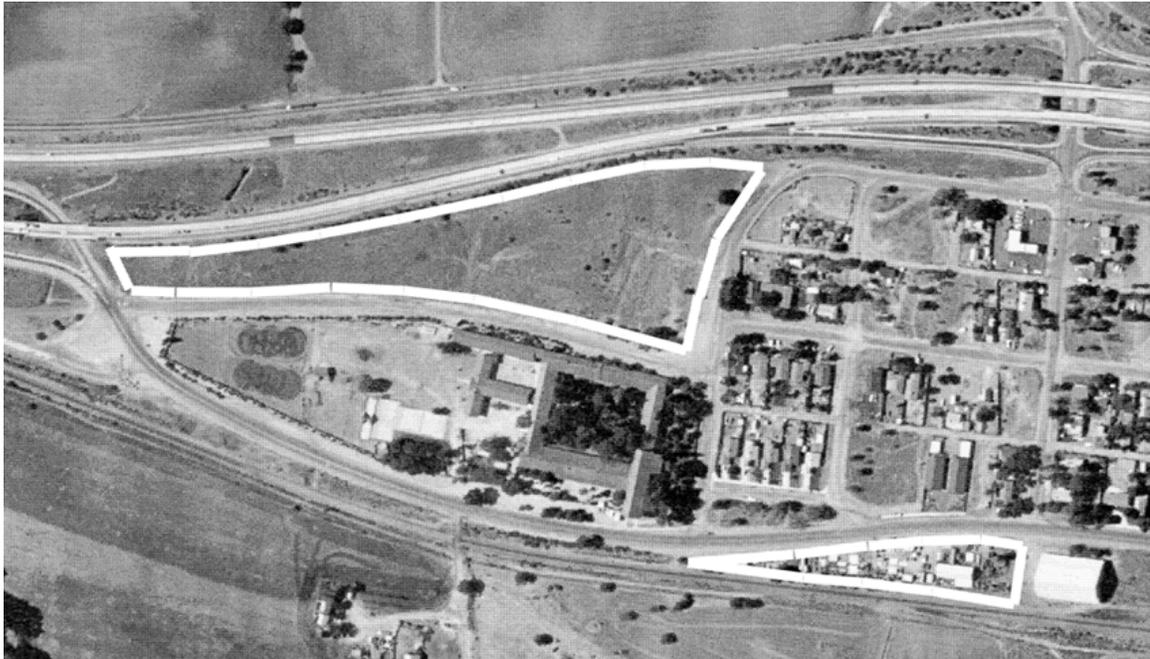
6. Improve the function and appearance of the Mission Street corridor and the physical and visual linkage between downtown and the mission.

Eliminate obstacles that would dissuade visitors to the mission from visiting downtown San Miguel.

- a. **Install sidewalks where needed along Mission Street.** The distance from the mission to downtown San Miguel is short enough (about 1/4 mile to 11th and Mission) that many visitors to the mission could be encouraged to walk downtown if a pleasant route were available. However, the absence of continuous sidewalks on the west side of Mission Street may discourage people from making the journey to downtown. New sidewalks in this area would normally be provided in conjunction with private development of adjoining parcels. Another possibility would be the creation of an improvement district with the specific purpose of constructing the sidewalk. It may also be possible to obtain outside funding, although public funding is usually reserved for sidewalks in the immediate vicinity of schools, parks and other public facilities.

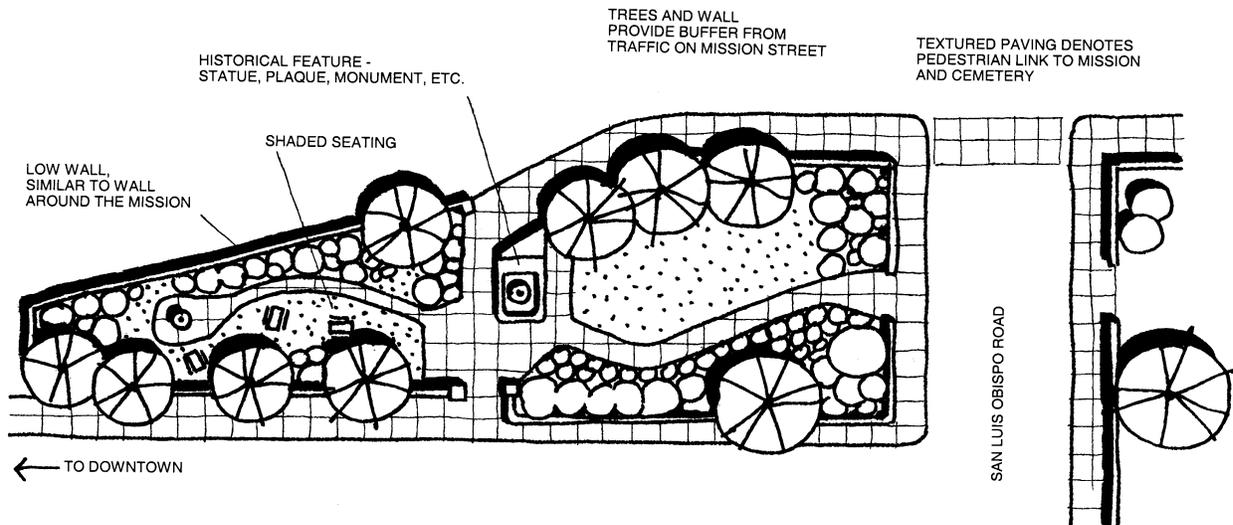
Existing healthy street trees should not be removed to accommodate new sidewalks. Instead, sidewalks should be designed with whatever off-sets are necessary to avoid the need for tree removal. In some locations, it may be necessary to locate a sidewalk in a setback on private property to accomplish this objective.

- b. Adopt site design and building design guidelines for development of properties in the immediate vicinity of the mission.** The land between the mission and Highway 101 provides the visual foreground for motorists viewing the mission from the highway. As vacant land, it allows an unobstructed view of the mission. However, development in this location has the potential to enhance or detract from the mission's visual setting. Guidelines are needed to ensure that future development on these parcels is compatible with the mission.



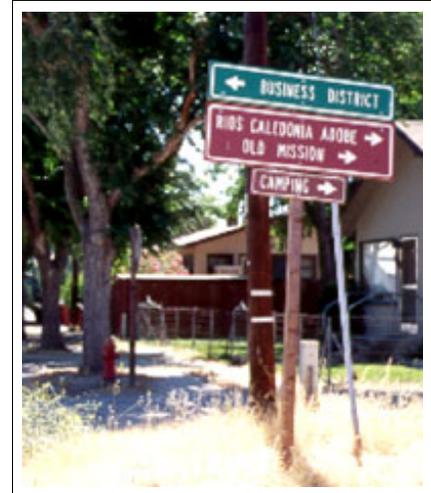
The present land use on the east side of Mission Street, immediately north of the mission, conveys no suggestion that there may be points of interest worth visiting elsewhere in San Miguel. This site dominates the view to the north from the entrance to the mission and the mission parking lot. Design guidelines for the downtown area should be extended to this parcel so that its future development will provide a more appealing link between the mission and the downtown.

Father Reginald park has the potential to become an extension of the mission experience for visitors. By emphasizing the park's historical associations, and with the addition of physical enhancements that would link it visually to the mission, it could become an important intermediate destination between the mission and the downtown. For example, sidewalks could be installed that use the same colored concrete as the sidewalks at the mission. A concept plan should be prepared to indicate how the park could be enhanced to accomplish this purpose.

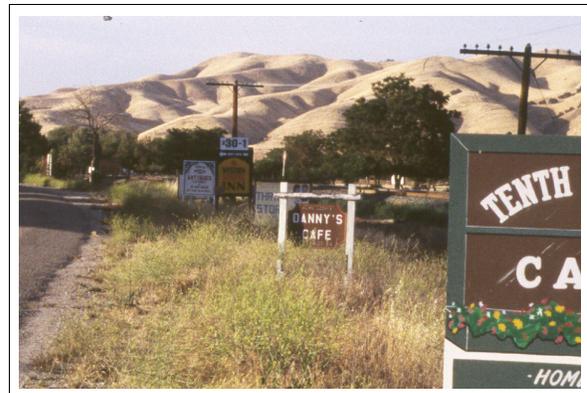


7. **Create or improve community “gateways” to convey a “visitor-friendly” message.**

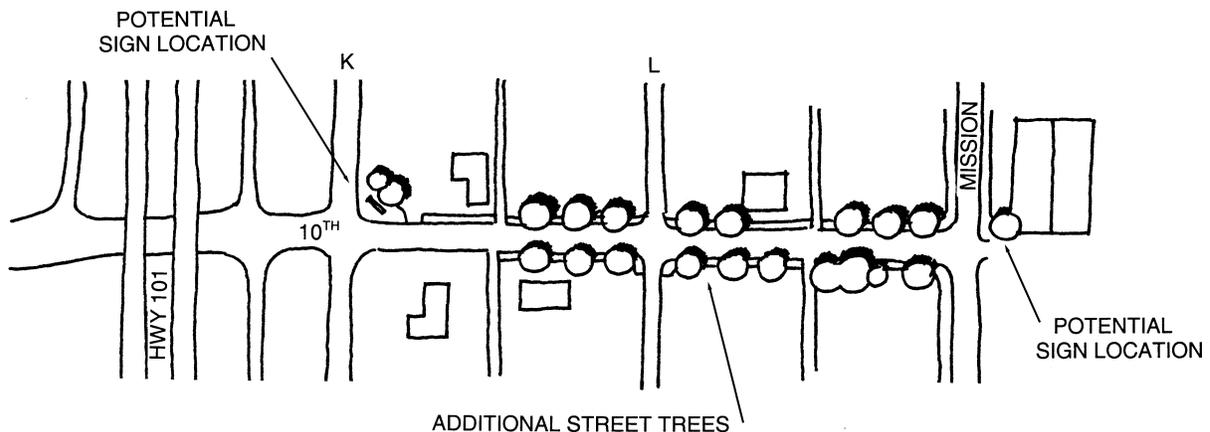
The principal points of arrival for motorists coming from Highway 101 are along Mission Street south of the mission and along 10th Street at the off-ramp. The southern approach is marked by a clutter of signs advertising individual local businesses. Tenth Street leads downhill to Mission Street, where a sign points south to the mission, without mentioning that “Historic Downtown San Miguel” is only a short distance to the north. These “gateways” help to establish the first impression visitors have of San Miguel. Care should be taken to ensure that this first impression is a positive one.



- a. **Replace individual business road signs with a community sign** to improve the visual approach to the community. The sign should contain a reference to San Miguel’s historic resources. The sign could be located across the street from and north of the mission, where it would be visible to motorists arriving at the Mission from the south. The county Land Use Ordinance allows community identification signs on each arterial street entering a community. Signs may have a maximum height of 12 feet and a maximum area of 100 square feet. Signs may indicate the types of visitor services available, but they may not include the names of individual businesses.



- b. Provide physical improvements and signage at 10th and Mission and 10th and Hwy 101.** Signage along 10th Street and at the intersection with Mission Street should call attention to downtown San Miguel as a place of historic interest. There could be a community sign listing local businesses, similar to the sign proposed for the southern approach. Installation of additional street trees along the length of 10th Street from the highway to Mission Street would also help to create a positive impression for visitors.



- c. Gateway arches.** Gateways could also be marked with an arch structure or a banner hung from supports on both sides of the street.
- d. Freeway signs should be considered.** Existing signs announce the mission and the Rios-Caledonia adobe. Freeway signs are also needed for “Historic San Miguel”. Preferred locations are south of the Mission Street exit for northbound traffic and north of the Tenth Street exit for southbound traffic. CalTrans should be consulted to determine the process for obtaining the necessary permits.

8. Prepare a Plan to Address Surface Drainage.

There are several locations throughout the community where storm water collects in the public right-of-way during the rainy season, creating inconvenience for motorists and pedestrians. Several factors contribute to this condition, including the absence of a consistent positive slope from the vicinity of L Street to the Salinas River, the railroad line, which acts as a barrier to natural flow from west to east, and the absence of curbs and gutters in many locations. As future development takes place, curbs and gutters will be constructed, leading to the correction of some of these conditions. A drainage study should be conducted to pinpoint the locations where problems currently exist, to determine how much of the problem is likely to be corrected over time by the construction of curbs and gutters, and whether there is some part of the problem that can only be solved by construction of a storm sewer or other capital improvement. Typically, funding for drainage projects is generated by annual assessments on properties within the boundaries of a drainage improvement district.

E. Implementation

Implementation of the San Miguel Community Design Plan will be accomplished in several ways:

- ✓ through adoption of standards and guidelines that establish the criteria for future development;
- ✓ through identification of programs - recommended actions or policies aimed at achieving the objectives of the community design plan;
- ✓ by specific initiatives such as the Town Center project.

Guidelines and standards. Guidelines are advisory and discretionary, indicating desirable qualities that a development proposal should include, unless there is a compelling reason not to do so. Standards are mandatory elements that must be included in any development. In the narrative description of standards and guidelines in this chapter, use of the words “shall” or “must” indicates a mandatory standard. Use of the word “should” indicates a guideline that is expected to be followed unless there is a compelling reason not to do so. It is the responsibility of the applicant to explain the circumstances that prevent the inclusion of a guideline in a proposed development.

The Design Plan’s guidelines and standards are not retro-active - that is, they do not apply to development existing at the time of adoption of the Design Plan. However, the county’s Land Use Ordinance provides that changes or additions to existing development may be required to comply with currently adopted standards. Some of these provisions are summarized below:

- ✓ *A land use permit is required for a project involving work on an existing building when the value of the improvement exceeds \$1500 or where a building expansion exceeds ten percent of the total floor area.*
- ✓ *Curb, gutter and sidewalk must be provided if the value of proposed alterations to a building exceeds 25 percent of the total value of all improvements on a parcel.*
- ✓ *Expansion of an existing use in the Central Business District (CBD) must provide required parking for the area of the expansion, only. Expansion of an existing use outside the CBD must provide required parking for the existing use as well as for the expansion.*

San Miguel Design Plan

✓ *A non-conforming single-family residence may be expanded up to 25 percent of the usable floor area without having to comply with newly adopted standards. Other non-conforming buildings must be brought into compliance if expanded.*

The Land Use Ordinance should be consulted for the exact wording and intent of these provisions.

1. Land Use Permit Requirement

A Minor Use Permit is a land use permit that is required in addition to a building permit. The intent of the Minor Use Permit is:

- ✓ to enable public review of significant land use proposals;
- ✓ to ensure the proper integration into the community of land uses which, because of their type or intensity, may only be appropriate on particular sites, or may only be appropriate if they are designed or laid out in a particular manner.

The Minor Use Permit process enables adoption of discretionary conditions of approval for new development projects. An environmental determination is made and the project is referred to the community advisory committee for review and comment. Action on a Minor Use Permit application may include:

- ✓ approval, based on the standards of the Land Use Ordinance and General Plan;
- ✓ approval with conditions;
- ✓ disapproval, based on conflict with provisions of the Land Use Ordinance and General Plan, or other pertinent information.

The Minor Use Permit process enables the use of standards and guidelines in the San Miguel Design Plan to ensure that new developments help to achieve community goals.

1.1 Permit Requirement

(Standard) Minor Use Permit approval is required for all new construction or exterior alteration of existing structures within the San Miguel Urban Reserve Line where a land use permit is required by Land Use Ordinance Article 2, except for the following:

- a. Minor exterior alterations, at the discretion of the Planning Director;
- b. Building expansions not exceeding 300 square feet, at the discretion of the Planning Director;

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- c. A new use proposed to occupy an existing development;
- d. Single-family residences, duplexes and residential accessory structures;
- e. Where Conditional Use Permit approval is otherwise required by the Land Use Ordinance or the General Plan;
- f. Projects in the San Lawrence Terrace neighborhood.

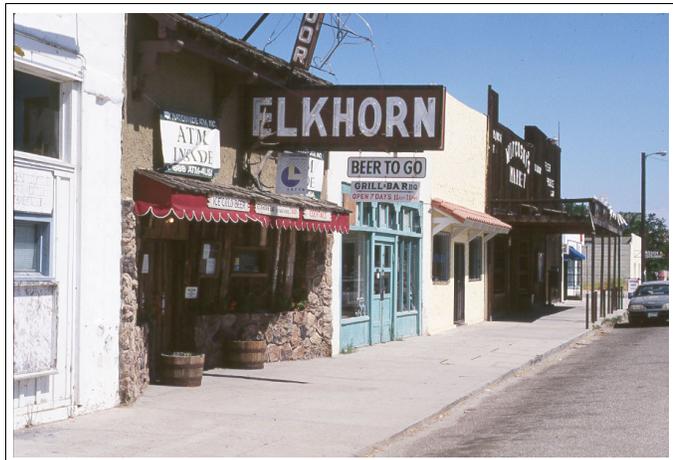
2. Guidelines and Standards for Projects in Downtown San Miguel, located on Mission Street between 11th Street and 14th Street.

The design plan makes a distinction between San Miguel's commercial, historic core area, on Mission Street between 11th and 14th Streets, and commercial areas on Mission Street outside of the core area. The intent of the guidelines and standards for the core area is to maintain and reinforce the historical development pattern, to assure an appropriate setting for the community's remaining original buildings and to encourage a level of pedestrian activity that will support local businesses.

2.1. Site Design / West Side of Mission Street

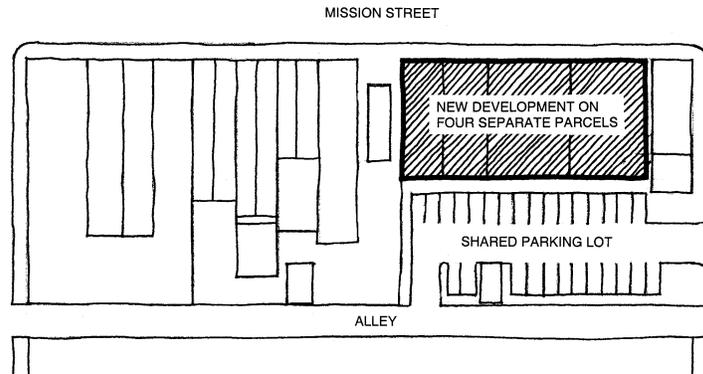
- a. **Front and side building setbacks.** (Standard) Front and side setbacks shall be zero, to maintain a visually interesting edge along the sidewalk and stimulate pedestrian activity. Exceptions:

A side set-back may be greater than zero to provide pedestrian access from a parking lot located at the rear of the lot. A front setback may be greater than zero if the area of the additional setback is usable by the public as an extension of the sidewalk or by customers as an outdoor dining area. The second floor may be set back from the Mission Street lot line to provide a porch or balcony overlooking the street.



- b. **Driveways.** (Standard) Where a development site has access to a rear alley or a side street, no driveway access will be allowed from Mission Street, in order to preserve the continuity of the Mission Street building facades, to avoid conflicts between vehicles and pedestrians, and to prevent the elimination of on-street parking spaces.
- c. **Parking layout.** (Guideline) The layout of parking spaces and parking circulation aisles should allow for vehicular circulation between adjacent parcels.

- d. **Reciprocal off-site parking agreement.** (Standard) Although a reciprocal parking agreement is not mandatory, if such an agreement is proposed, the applicant must submit documentation of an agreement to share parking and circulation with owners of adjacent parcels. Documentation is defined as a recorded lease in a form approved by County Counsel, per LUO section 22.18.070.

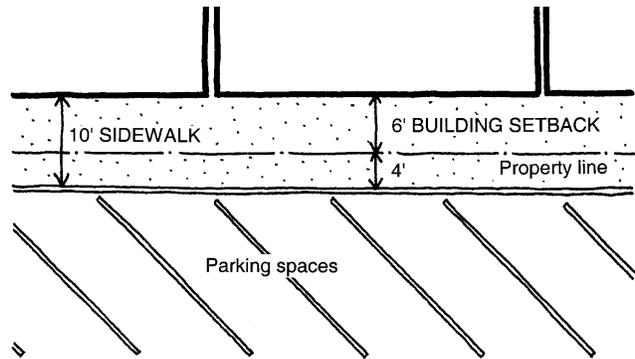


- e. **Rear building setback.** (Guideline) The rear building setback should be sufficient to allow an efficient use of the site for parking and circulation in conjunction with the parking and circulation layout on adjoining parcels, as indicated in the diagram, above.
- f. **Uses allowed in rear setback.** (Standard) The rear setback may be used only for parking spaces, parking access aisles, for an enclosed area for temporary collection of solid waste per LUO section 22.10.150 and for accessory buildings and outdoor storage. (Guideline) The arrangement of these uses on the site should consider the implementation of a parking and circulation concept coordinated with development on adjoining parcels.
- g. **Required parking.** (Standard) Parking is required for all residential uses per LUO Chapter 22.04 and for all new commercial uses at a ratio of 1 parking space per 1000 gross square feet of commercial building space. Parking requirements are waived for present or proposed uses of existing commercial buildings located on the west side of Mission Street, between 11th and 16th Streets. The purpose of this waiver is to encourage retention and productive use of these historic buildings.

- h. Exemption.** (Standard) Parcels with less than 50 feet alley frontage where shared parking is infeasible are exempt from provisions 2.1.c through g, above, except for the requirement to provide parking for residential use, either on site or off-site with a recorded lease agreement.

2.2. Site Design / East Side of Mission Street

- a. Front and side building setbacks.** (Standard) Minimum front setback is 6 feet from the property line. This setback accounts for the existing sidewalk location and for the need to accommodate angled parking, along with a sidewalk that is wide enough to allow for anticipated pedestrian activity. Where sidewalks presently exist, the curb location is four feet inside the Mission Street right-of-way. To provide for a ten-foot wide sidewalk, the east edge of new sidewalks will be located six feet east of the right-of-way boundary, with the west edge of the sidewalk maintaining the present alignment, four feet inside the right-of-way.



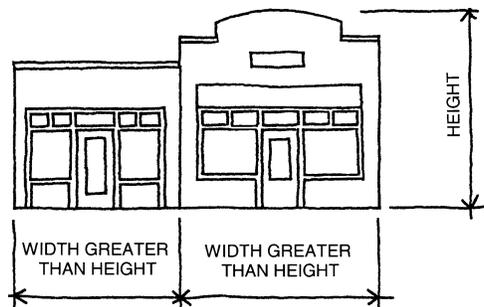
Side setbacks shall be zero. Exceptions: A front setback may be greater than six feet if the area of the additional setback is usable by the public as an extension of the sidewalk. A side setback may be greater than zero to provide pedestrian access from a parking lot located at the rear of the lot.

- b. Rear building setback.** (Standard) 5 feet. If it becomes available in the future, 30 feet of additional parcel depth will allow development of a one-way parking aisle with one row of angled parking. The additional 5 foot setback provides a buffer between the parking spaces and the rear facade of the building.
- c. Uses allowed in rear setback.** (Standard) Parking, sidewalk, enclosed area for temporary collection of solid waste.

- d. **Required parking.** (Standard) Parking is required for all new commercial uses at a ratio of 1 parking space per 1000 gross square feet of commercial building space.
- e. **Parking layout.** (Guideline) Site design may anticipate the future availability of additional site area to the rear of existing parcels by considering the parking spaces required by these standards to be a temporary use, to be replaced by additional parking spaces located in the expanded site area.
- f. **Reciprocal off-site parking agreement.** (Guideline) If parcel depth is increased through acquisition of additional property as noted above, an applicant should submit documentation of an agreement to share parking and circulation with owners of contiguous parcels. Documentation is defined as a recorded lease in a form approved by County Counsel, per LUO section 22.18.070.

2.3. Building Height and Proportion

- a. **Building height.** (Standard) Maximum allowable building height is two stories. The allowable building height is expressed in stories rather than in feet to provide design flexibility within a framework that respects the historic scale of development.
- b. **Building proportion.** (Guideline) The height of a building's Mission Street facade should be no greater than the building's width, in order to maintain compatibility with historical building proportions in downtown San Miguel. Articulation of the facade should include no portion designed to emphasize its verticality.
- c. **Roofs.** (Guideline) Roof forms should be consistent with existing commercial buildings in the core area - shallow-pitch gable roofs, usually with a parapet wall facing the street.

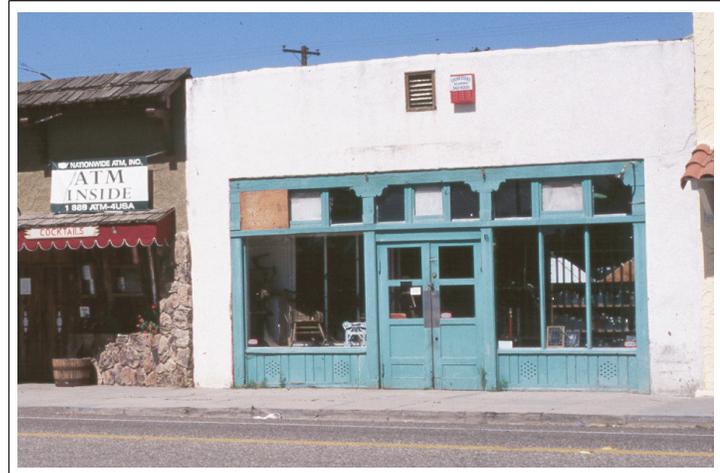


2.4. Pedestrian Scale Details

- a. **Facade articulation.** (Guideline) Building design should include cornices, window moldings, parapet caps, overhangs, awnings and other three-dimensional architectural features that cast shadows and create visual interest.

- b. **Windows and doors.** (Guideline) Building facades should have plenty of windows and door glazing to provide passing pedestrians with a view of merchandise displays and customers inside. Between 60 percent and 85 percent of the ground level street facade should be transparent glass (windows and doors).

This facade is about 65 percent transparent



- c. **Recessed building entries.** (Guideline) Entries should be recessed so that doors do not open into the path of pedestrians and to create maneuvering space for pedestrians entering and leaving the building.
- d. **Building materials encouraged.** (Guideline) Building materials that evoke the design themes of the mission or of the early railroad era are encouraged. These materials include natural finish or painted wood, stucco, unpainted brick, wood window frames and moldings and mission tile roofs.
- e. **Building materials prohibited.** (Standard) Building materials that are not consistent with the mission or early railroad design themes are not allowed. These materials include exposed concrete block, highly reflective surfaces (other than windows and doors), reflective glass, glass block, metal siding, painted brick, plastic and unpainted/un-anodized aluminum.
- f. **Signs.** (Guideline) Signs should be designed to be consistent with the mission, early railroad or pre-1950's design themes. Signs should not be made of plastic. They should not be internally lighted, except that they may include neon tubing.

Signs should be directly illuminated with building-mounted light fixtures. Signs mounted perpendicular to the building facade are encouraged, because they are more readily visible to approaching motorists and pedestrians.

- g. Pole signs.** (Standard) Pole signs are not allowed.
- h. Street trees.** (Standard) New development shall include its proportionate share of the number of street trees needed to achieve a continuous row along the Mission Street frontage with spacing of 30 feet between trees. Each development plan should include at least one street tree. Plans for parcels with frontage exceeding 30 feet shall include one street tree per 30 feet of frontage or fraction thereof.
- i. Building lighting.** (Guideline) Exterior light fixtures should be designed and adjusted to direct light away from any road, street or dwelling unit.

3. Guidelines and Standards for Projects located on Mission Street south of 11th Street and north of 14th Street.

The intent of the guidelines and standards for development on Mission Street outside of the core area is to contribute to the positive image of San Miguel through building design that is compatible with the community's historic buildings, to develop and maintain an adequate supply of on-site parking and to encourage a level of pedestrian activity that will support local businesses. ***Guidelines and standards are the same as for the core area, with the following exceptions:***

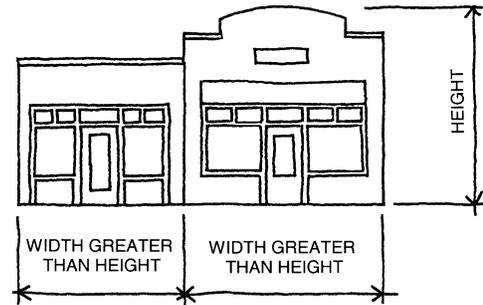
3.1 Site Design

- a. **Building setbacks.** (Standard) Building setbacks are as specified in the *Land Use Ordinance*, section 22.10.140 et seq.
- b. **Required parking.** (Standard) On-site parking requirements, as specified in the county Land Use Ordinance, may be reduced in proportion to the supply of street parking available in the vicinity of a proposed development. For example, in blocks where diagonal parking is available, the allowable reduction would be greater than elsewhere. On-site parking requirements may be reduced if a development is designed to share parking spaces with adjoining parcels.
- c. **Parking location.** (Guideline) Parking spaces provided on-site should be located at the rear of the parcel. Parking may be located at the side of the parcel with a solid wall along the street property line to maintain the continuity building facades at the edge of the sidewalk.

3.2. Building Height and Proportion.

- a. **Building height.** (Standard) Maximum allowable building height is two stories. The allowable building height is expressed in stories rather than in feet to provide design flexibility within a framework that respects the historic scale of development.

- b. **Building proportion.** (Guideline) The height of a building's Mission Street facade should be no greater than the building's width, in order to maintain compatibility with historical building proportions in downtown San Miguel. Articulation of the facade should include no portion designed to emphasize it's verticality.



- c. **Roofs.** (Guideline) Roof forms should be consistent with existing commercial buildings in the core area - shallow-pitch gable roofs, usually with a parapet wall facing the street.

3.3. Pedestrian Scale Details.

- a. **Facade articulation.** (Guideline) Building design should include cornices, window moldings, parapet caps, overhangs, awnings and other three-dimensional architectural features that cast shadows and create visual interest.
- b. **Windows and doors.** (Guideline) Building facades should have plenty of windows and door glazing to provide passing pedestrians with a view of merchandise displays and customers inside. Between 60 percent and 85 percent of the ground level street facade should be transparent glass (windows and doors).
- c. **Recessed building entries.** (Guideline) Entries should be recessed so that doors do not open into the path of pedestrians and to create maneuvering space for pedestrians entering and leaving the building.
- d. **Building materials encouraged.** (Guideline) Building materials that evoke the design themes of the mission or of the early railroad era are encouraged. These materials include natural finish or painted wood, stucco, unpainted brick, wood window frames and moldings and mission tile roofs.
- e. **Building materials prohibited.** (Standard) Building materials that are not consistent with the mission or early railroad design themes are not allowed. These materials include exposed concrete block, highly reflective surfaces (other than windows and doors), reflective glass, glass block, metal siding, painted brick, plastic and unpainted/un-anodized aluminum.

- f. **Signs.** (Guideline) Signs should be designed to be consistent with the mission, early railroad or pre-1950's design themes. Signs should not be made of plastic. They should not be internally lighted, except that they may include neon tubing. Signs should be directly illuminated with building-mounted light fixtures. Signs mounted perpendicular to the building facade are encouraged, because they are more readily visible to approaching motorists and pedestrians.
- g. **Pole signs.** (Standard) Pole signs are not allowed.
- h. **Street trees.** (Standard) New development shall include its proportionate share of the number of street trees needed to achieve a continuous row along the Mission Street frontage with spacing of 30 feet between trees. Each development plan should include at least one street tree. Plans for parcels with frontage exceeding 30 feet shall include one street tree per 30 feet of frontage or fraction thereof.
- i. **Building lighting.** (Guideline) Exterior light fixtures should be designed and adjusted to direct light away from any road, street or dwelling unit.

4. Guidelines and Standards for Projects located in the Industrial land use category.

4.1. Building Height and Proportion

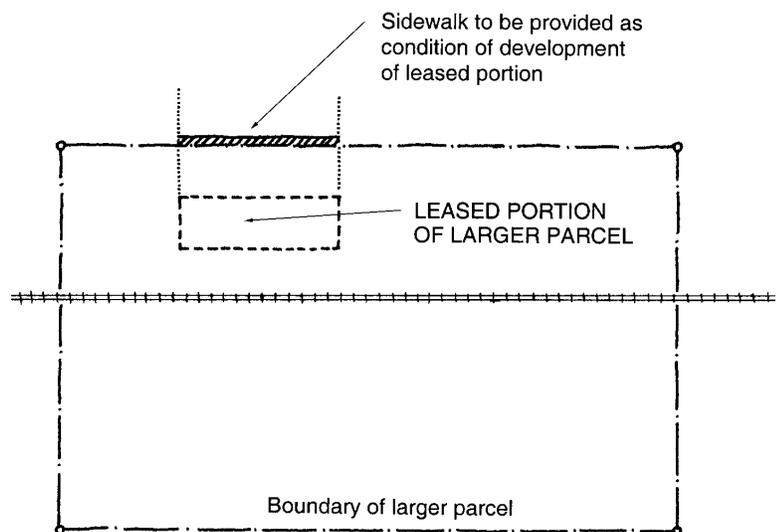
- a. **Building height.** (Standard) Maximum allowable building height is 35 feet for projects located south of 14th Street and north of 11th Street.
- b. **Building proportion.** (Guideline) The width of a building's street facade should be equal to or greater than the building's height, in order to maintain compatibility with historical building proportions in San Miguel. Articulation of the facade should include no portion designed to emphasize it's verticality. For development on parcels with frontage equal to or greater than 50 feet, building facades should be subdivided into bays to achieve compatibility with historical proportions.

4.2. Pedestrian Scale Details

- a. **Facade articulation.** (Guideline) Building design should include cornices, window moldings, parapet caps, overhangs, awnings and other three-dimensional architectural features that cast shadows and create visual interest.
- b. **Recessed building entries.** (Guideline) Entries should be recessed so that doors do not open into the path of pedestrians and to create maneuvering space for pedestrians entering and leaving the building.
- c. **Windows and doors.** (Guideline) Building facades should have windows and doors. If such openings are not appropriate for the building's function, the design should include features that establish a pattern or rhythm similar to a continuous retail facade that is regularly punctuated with windows and doors.
- d. **Building materials encouraged.** (Guideline) Building materials that evoke the design themes of the mission or of the early railroad era are encouraged. These materials include natural finish or painted wood, stucco, unpainted brick, wood window frames and moldings and mission tile roofs.
- e. **Building materials discouraged.** (Guideline) Building materials that are not consistent with the mission or early railroad design themes are discouraged. These materials include exposed concrete block, highly reflective surfaces (other than

windows and doors), reflective glass, glass block, metal siding, painted brick, plastic and unpainted/un-anodized aluminum

- f. **Signs.** (Guideline) Signs should be designed to be consistent with the mission, early railroad or pre-1950's design themes. Signs should not be made of plastic. They should not be internally lighted, except that they may include neon tubing. Signs should be directly illuminated with building-mounted light fixtures.
- g. **Pole signs.** (Standard) Pole signs are not allowed.
- h. **Curbs, gutters and sidewalks.** (Standard) Curbs, gutters and sidewalks are required for all development in the Industrial land use district. Development on leased land which is part of a larger parcel, such as the railroad parcels, is subject to this requirement. If a leased parcel does not directly adjoin a street property line, its responsibility for constructing curb, gutter and sidewalk shall be based on the length of the parcel measured along its side that is most nearly parallel with the center line of the street.



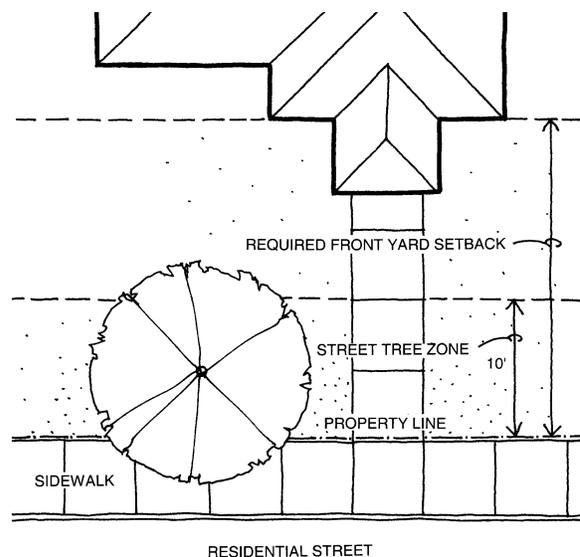
- i. **Planting strips.** (Guideline) Sidewalks should be separated from the curb by a minimum three-foot wide planting strip or parkway, to accommodate street trees.
- j. **Street trees.** (Standard) New development shall provide deciduous shade trees at a spacing of 30 feet or fraction thereof. The Countywide Design Plan contains a list of species approved for use as street trees.
- k. **Building lighting.** (Guideline) Exterior light fixtures should be designed and adjusted to direct light away from any road, street or dwelling unit.

5. Guidelines and Standards for Projects Located in the Residential Multi-Family (RMF) Land Use Category

5.1. Site Design

The site design of multi-family developments should incorporate design concepts associated with single-family neighborhoods.

- a. **Building size.** (Standard) To achieve compatibility with adjoining single-family neighborhoods, no more than six dwelling units are allowed in any single structure.
- b. **Use of front yard setback.** (Guideline) To achieve compatibility with adjoining single-family neighborhoods, the space between buildings and the main street frontage should not be used as a location for required on-site parking spaces. Parking areas should be located within the interior of the site or on the sides and rear where they can be adequately screened from view by fencing and landscaping.
- c. **Private open space.** (Guideline) Each unit in an RMF development should include at least 150 square feet of private usable open space, in the form of decks or ground level patios.
- d. **Shared open space.** (Standard) Multi-family developments must include shared usable open space which may include such features as gardens, play areas or swimming pools.
- e. **Sidewalks.** (Standard) Curbs, gutters and sidewalks are required for all new development in the Residential Multi-Family land use district.
- f. **Street trees.** (Standard) New development in the RMF category must include at least one deciduous shade tree per 30 feet of frontage or fraction thereof, planted within 10 feet of the front property line. The Countywide Design Plan contains a list of species approved for use as street trees.



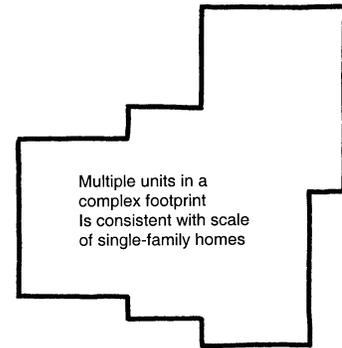
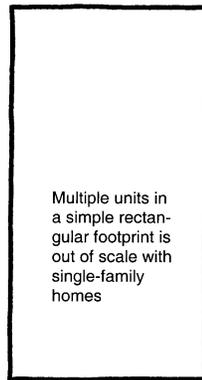
- g. Building lighting.** (Guideline) Exterior light fixtures should be designed and adjusted to direct light away from any road, street or dwelling unit.

5.2. **Building Design**

Multi-family buildings should be designed to include details and features similar to single-family homes.

- a. Entries.** (Guideline) Each unit of a multi-family development should have its own separate main entry from the outdoors. For any building having frontage on a public street, entries to the units should face the street.

- b. Building footprint.** (Guideline) Building footprints should resemble a series of partially overlapping rectangles, as opposed to a single rectangle.



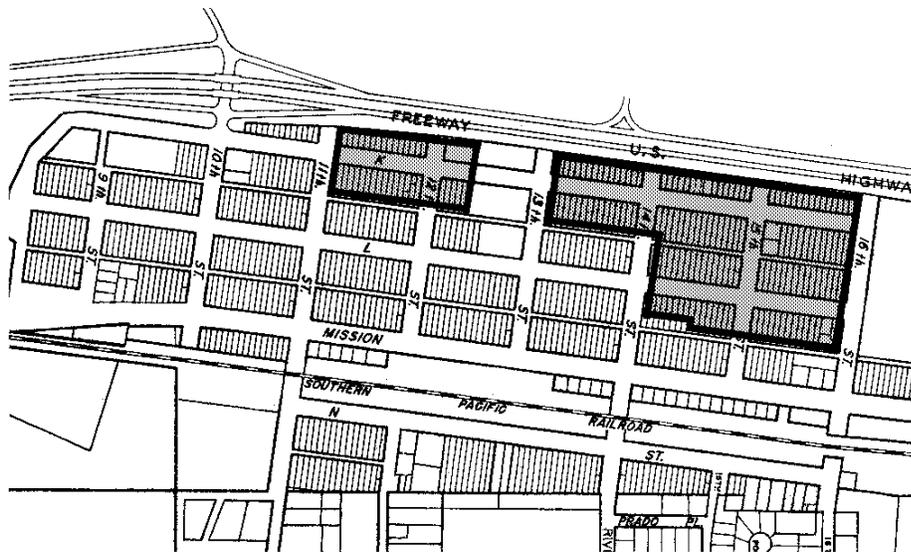
- c. Building details.** (Guideline) Buildings should include details associated with single-family homes, such as porches, bay windows, chimneys, trellises, built-in planters, integrated low walls. Each dwelling unit should be made individually recognizable through the use of balconies, porches, setbacks or other architectural features.
- d. Building height.** (Standard) Maximum allowable building height is two stories.

6. Guidelines and Standards for Projects Located in the Residential Single Family (RSF) Land Use Category

6.1. Site Design

The site design of single family developments should be consistent with the design concepts of this community design plan.

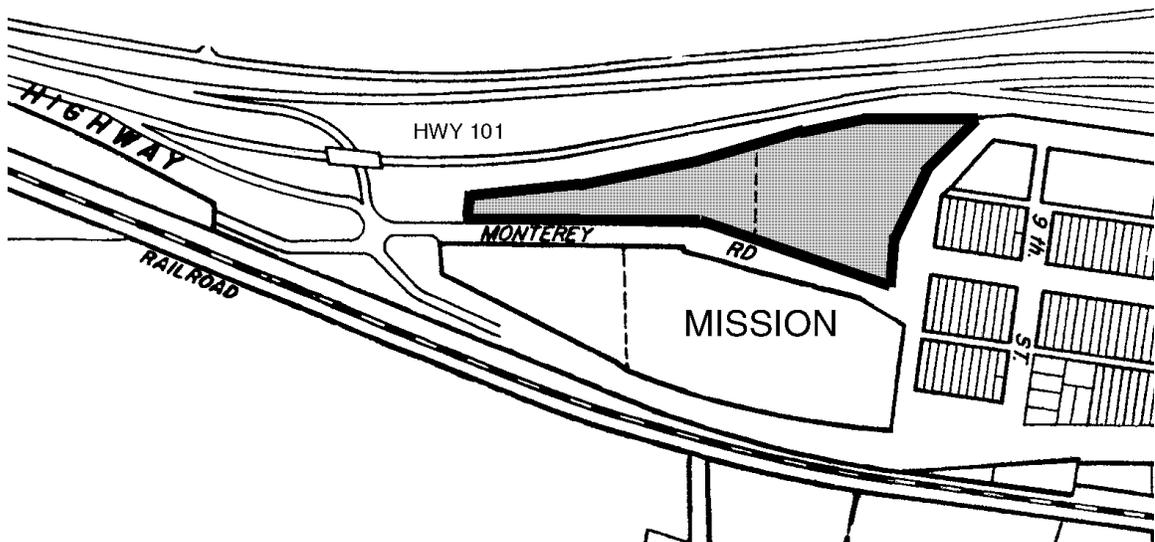
- a. **Curbs, gutters and sidewalks.** (Standard) Curbs, gutters and sidewalks are required for all development in the Residential Single Family land use district, except for parcels within the area identified in the exhibit below. This area contains many historic buildings which the community hopes to retain. If sidewalks were required for infill development in this area, the result would be a patchwork of isolated sidewalk segments. To avoid this consequence, sidewalks are not required in this area.



- b. **Street trees.** (Guideline) Streets in residential areas should include at least one deciduous shade tree per 30 feet of frontage or fraction thereof, planted within 10 feet of the front property line. See the diagram on page 45.
- c. **Parking.** (Standard) New single-family residential development with an alley as its primary access is required to have two on-site parking spaces in addition to those normally required, in order to compensate for the absence of street parking.

7. Guidelines and Standards for Projects Located Near Mission San Miguel

(Guideline) Development between Highway 101 and the mission should not detract from views of the mission buildings from the freeway. Building height, roof design, roofing materials and signage are the most important considerations. Building height should be limited to one story, buildings should have gable roofs and mission tile, and pole signs should not be allowed. These guidelines apply to the parcels identified in the exhibit, below.



7. Programs

This table identifies summarizes those strategies from chapter D that have not otherwise been implemented as guidelines or standards. These programs should be implemented over time based on the availability of human and financial resources.

San Miguel Programs						
#	Program	Responsibility	Potential Funding			Timing
			Local	County	State/ Fed	
1A	Enforcement: Land Use & Health	County		N/A		ongoing
1B	Leaflet: "Good Neighbors Checklist"	Community organizations with county assistance	✓	✓		2003
1C	Clean-Up Program	Community organizations with county assistance	✓	✓		ongoing
4A	Town Center Project	Community and County	✓	✓	✓	2002-10
4B	Mission St. Traffic Calming	County (included in Town Center Project)	✓	✓	✓	2003-05
5A	Identify Historic Resources	Community organizations with county assistance	✓			2003
5B	Historic Walking Tour	Community organizations	✓			2003
5C	Historic Loan Program	Community organizations with county assistance	✓		✓	2003-04
6A	Mission St. Sidewalks	Community and County	✓		✓	ongoing
6B	Fr. Reginald Park Enhancement	Community and County	✓			2004-08
7A	South Gateway	Community organizations with county assistance	✓			2004-06
7B	Tenth St. Gateway	Community organizations with county assistance	✓			2004-06
7D	Freeway Signage	Community and County	✓		✓	2003-04
8	Drainage Inventory	County	✓	✓	✓	2003-04
9	Alley Standards	County		N/A		2004-05